



Republika ng Pilipinas
KAGAWARAN NG PAGAWAIN AT LANSANGANG PAMBAYAN
TANGGAPAN NG KALIHIM
Maynila

097.13 DPWH
04-24-2003



DEPARTMENT ORDER)

NO. **96**)
SERIES OF 2003)

APR 23 2003

SUBJECT : Provision of Informational Signs During
Inspection/Verification of On-going and
Completed DPWH Projects and
Application of Red Paint Marking on
Defective Works to Be
Replaced/Reworked by Contractors

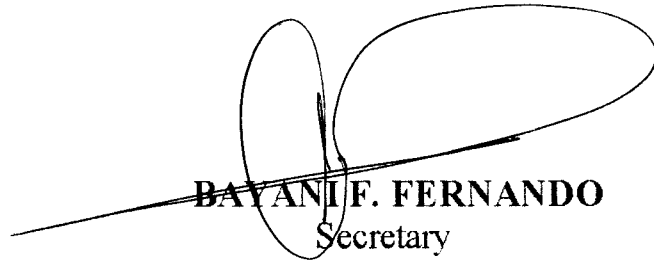
It has been observed that the public is not properly informed of the basic quality assurance activities being undertaken by inspectors during inspection/verification of on-going and completed projects such as test pittings to check the thickness and quality of the underlying soil components of the road as well as core boring test for thickness and strength determination of concrete and asphalt pavements, among others. While the intention of the inspectors is to assure that the projects are constructed in accordance with the approved plans and specifications by undertaking destructive testing in some cases, the general public who literally pays for those projects oftentimes misconstrues such procedures as otherwise, due to the absence of appropriate informational signs for the purpose.

Additionally, defective structures such as concrete and asphalt roads requiring immediate removal and replacement are the most visible to the public eye and may create a bad image to the Department if the public will not be informed properly.

Henceforth, it is hereby directed that all portions of defective concrete and asphalt pavements including bridge slabs and approaches requiring removal and replacement shall be sprayed/applied with red paint marking as determined by inspectors, following the procedure shown in "Attachment 1" of this Order.

Likewise, all implementing offices are directed to provide the necessary informational signs to be used by inspectors during the course of their project inspection/assessment as shown in "Attachment 2", also of this order, and shall serve as basis for all other informational signs to be provided by the implementing offices in the implementation of all construction and maintenance projects of the Department.

For strict compliance.



BAYANI F. FERNANDO
Secretary

PROCEDURE FOR the APPLICATION of RED PAINT MARKING
ON DEFECTIVE WORKS OF DPWH PROJECTS

1. Red paint marking shall apply only on the following defective structures requiring removal and replacement:
 - 1.1 Concrete pavement
 - 1.2 Asphalt pavement
 - 1.3 Bridge approach
 - 1.4 Bridge slab
2. An "X" mark (1.5 m x 1.5m) shall be sprayed/applied at the location shown in the sketch below indicating therein the limit or area to be removed and replaced. The defective area shall be inscribed with the word "DPWH REJECTED" with letterings of about 15 cm high at all times.
3. If the road is already open to vehicular traffic, the marking shall be properly protected and allowed to dry before the pavement is opened to traffic again.

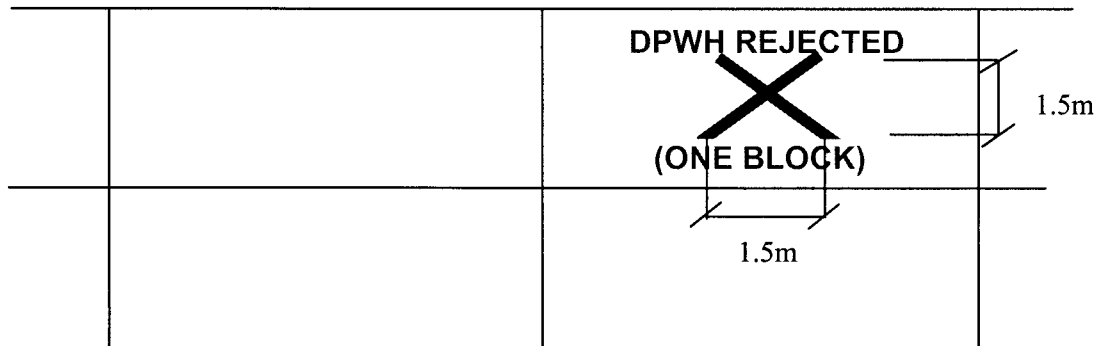


Figure 1a. In the case of concrete pavement, indicate number of blocks rejected.

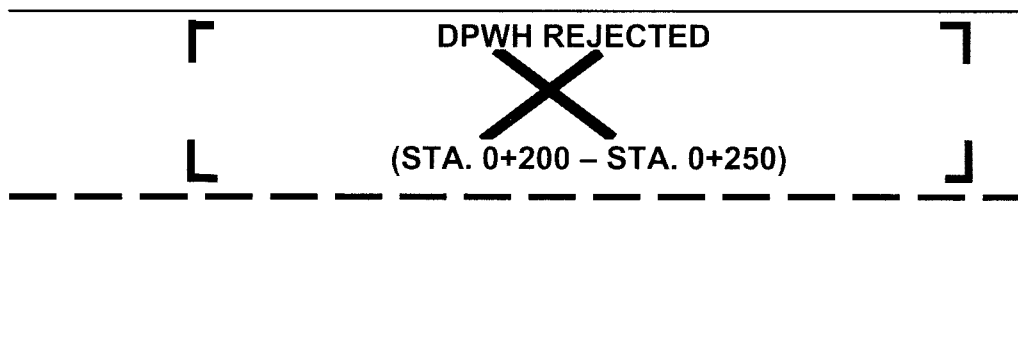


Figure 1b. In the case of asphalt pavement, indicate the station limits of the rejected area.

INFORMATIONAL SIGNS

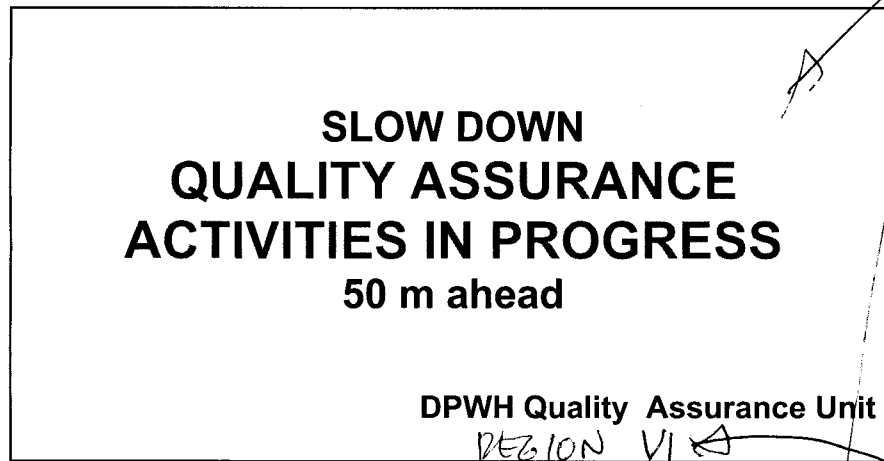


Figure 2a. To be used during test-pitting of base/subbase and checking of actual dimensions of on-going and completed structures.

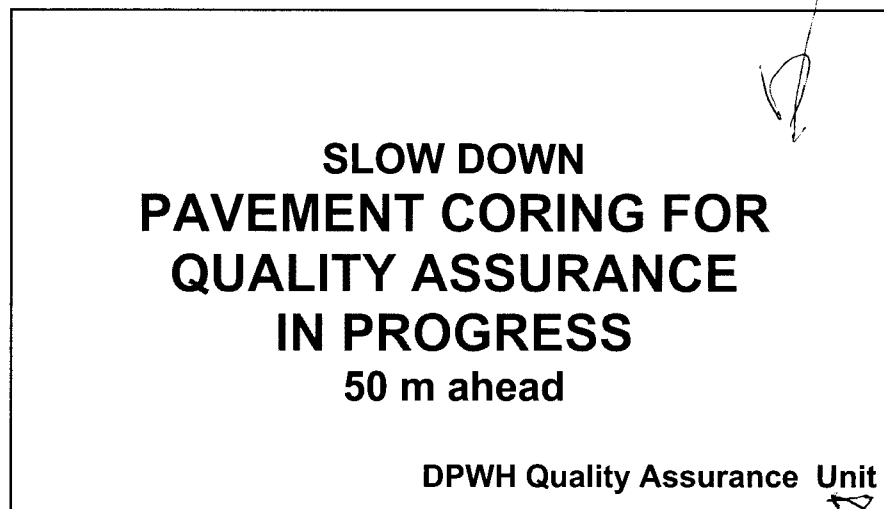


Figure 2b. To be used during the conduct of coring for thickness and strength determination of concrete and asphalt pavements

UNIT UNDERTAKING
INSPECTION
MAY BE
CENTRAL OFFICE
ETC.

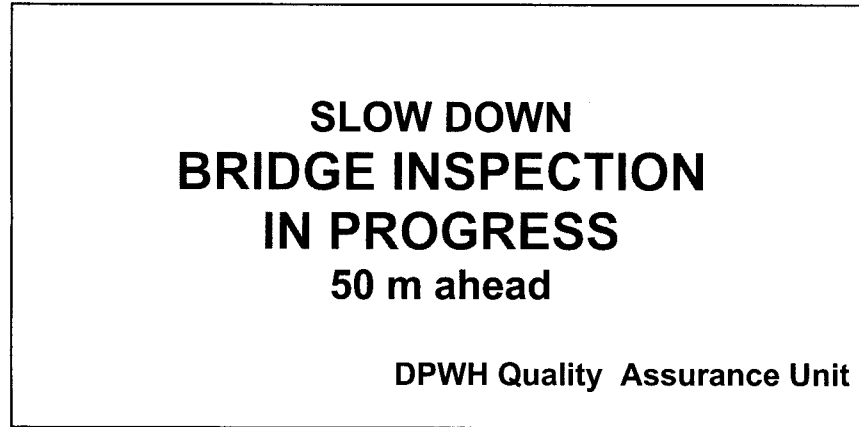
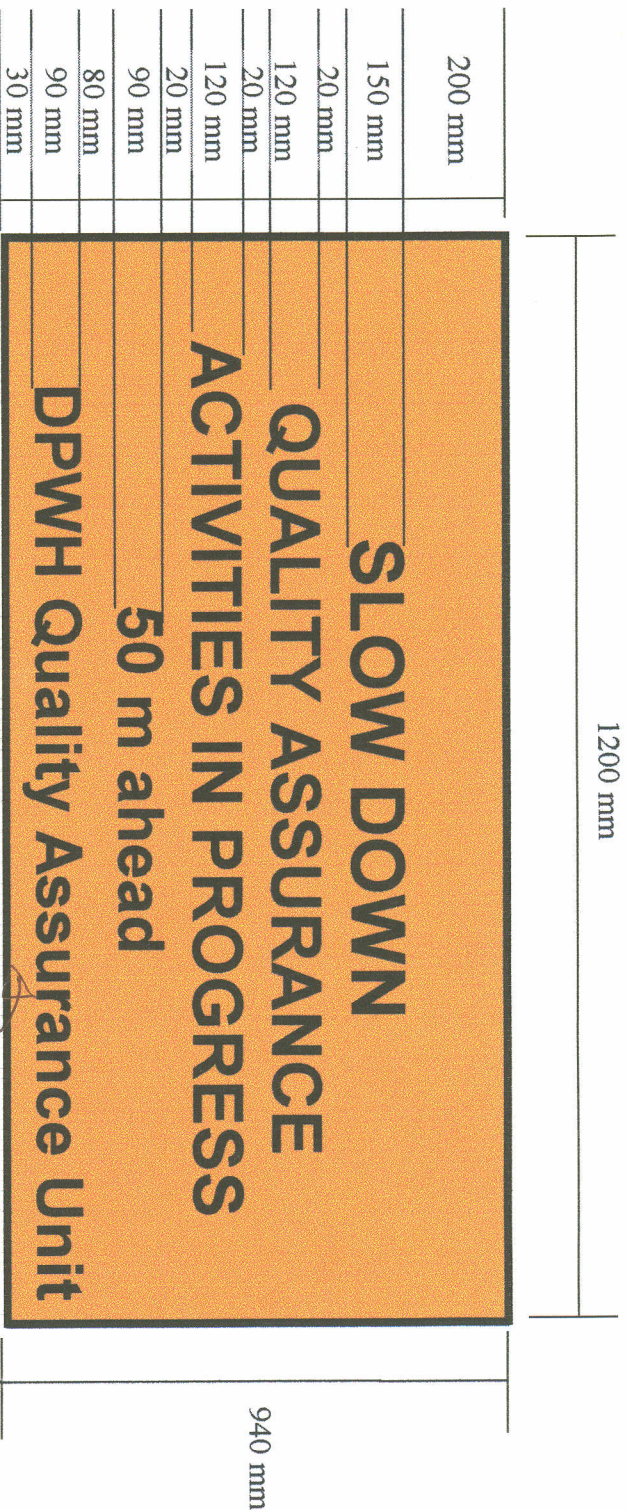


Figure 2c. To be used during actual verification of on-going and completed bridge components and other related structures

TYPICAL WARNING/INFORMATIONAL SIGN



Material to be used : Marine Plywood
 Background : California Orange
 All letterings : Black

Not drawn to scale

50 m ahead UNIT

Note: Signs generally should be located on the right side of the road where a driver would expect to find them. In some circumstances, the sign may be mounted above the carriageway. In rural areas, signs should be located not less than 75m nor more than 200m of the work area. In urban areas, signs should be located not less than 30m nor more than 100m of the work area. Supports/Stands for signs may be constructed of metal or timber. On high speed roads, the use of breakaway or collapsible supports should be considered for roadside signs where they are likely to be struck by vehicles. Sign faces may be constructed of plain aluminum sheets at least 3mm in thickness.